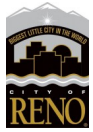


City of Reno Downtown Streetscape Design Standards

*(Supersedes *Redevelopment District No. 1 Streetscape Master Plan*)*

2nd Reading

January 08, 2020



Downtown Streetscape Design Standards

Contributors

CITY COUNCIL

Hillary Schieve; Mayor
Naomi Duerr; Vice-Mayor, Ward 2
Jenny Brekhus; Ward 1
Oscar Delgado; Ward 3
Neoma Jardon; Ward 5
Devon Reese; At Large
Bonnie Weber; Ward 4

STAKEHOLDERS

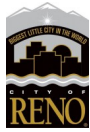
Cynthia Albright, AICP, CUD, GISP; Stantec
Jonathan Boulware; Jacobs Entertainment
Cindy Carano; Eldorado and “The Row” properties
Amy Cummings, AICP; Reg. Transportation Comm.
Dave Darrenogue; Reno Public Works Department
Lee Dillard; Harrahs Entertainment
Heidi Gansert; University of Nevada Reno
Britton Griffith; Downtown Merchants Assoc
Garrett Gordon; Jacobs Entertainment
Daniel Kovach, ASLA; Atkins Global
Ken Krater, PE; Downtown Alliance
Troy Miller; University of Nevada Reno
Eric Olson; Whitney Peak Hotel
Tray Palmer; Reno Fire Department
Barb Santner, ASLA; Stantec
Jeff Siri; Calneva
Alex Stettinski; Downtown Reno Partnership
Kurt Stitser; The Eddy
Bill Thomas, AICP; Assistant City Manager
Par Tolles; Tolles Development
Jana Vanderhaar, ASLA; Verdant Connections LA
Rebecca Venis; Neighborhood Services
Byron “Mac” Venzon; Reno Police Department
Alex Woodley; Neighborhood Services

PROJECT MANAGER

Aric Jensen, AICP; Office of Econ. Development

TECHNICAL TEAM

Matt Basile, CA; Parks and Recreation
Alexis Hill; Office of Arts, Culture, and Events
Charla Honey, PE; Public Works
Rishma Khimji; Information Technology
Kerri Koskie, PE; Public Works
Jeff Limpert; Office of Econ. Development
Jeff Mann; Parks and Recreation
Brook Oswald, PLA; Community Development
Khalil Wilson, PE; Public Works



Downtown Streetscape Design Standards

Process and Procedures

Introduction

In 1996, the City of Reno Redevelopment Agency created the Redevelopment District No 1. Streetscape Master Plan in an effort to “improve upon the image of the downtown in general”. The Plan was subsequently re-evaluated and updated in 2007. The new Downtown Streetscape Design Standards essentially reconfigures the 2007 Revision into a more succinct and user friendly document with updated construction details based on industry standards and the vision of community stakeholders.



The 2019 update specifically includes a more robust set of values and principles, a revised tree well design, a tree canopy standard, a shift from stamped concrete to a jointed grid, the removal of the coral-colored back-of-curb strip, and a more clearly defined project area. It also contains a new “Special Standards Corridor” overlay map which designates a sub-area for the possible future installation of infrastructure and street elements designed specifically to enhance special events and other aspects of the Virginia Street Corridor.

Concept and Desired Outcome

The primary purpose of the Downtown Streetscape Design Standards is to create a physical environment that is conducive to positive public activity, including both daily and special events. In an urban community the streetscape typically includes side-

walks, utility boxes, lighting fixtures, trees and shrubs, traffic signals, benches, trash receptacles, mailboxes, signs, decorative elements/art, bike lanes, and transit stops. However, unlike other sections of the community, a portion of downtown Reno is also frequently utilized for special events and so the streetscape may also include non-typical elements such as power pedestals, in-street bollards, public wi-fi hotspots, and related event infrastructure.

As part of the design process, the technical team researched existing streetscape configurations in similar communities, particularly in the Intermountain West, and incorporated elements from those streetscapes that are appropriate for Reno.

Values and Principles

All of the standards and regulations contained herein were evaluated against the following value statements recommended by both the Technical and Stakeholder Committees:

Principle 1.0 Development standards should be clear and easy to understand

1.1 The Downtown Streetscape Design Standards will be utilized by multiple departments within the City as well as outside agencies and private property owners, and it is important to its success that it be understood by all users and contain as little ambiguity as possible.

1.2 The Design Standards should have a simple, clear process for addressing situations where exact compliance is not possible, similar to the hardship provisions of a zoning variance.

1.3 The Design Standards should emphasize industry standard diagrams and details whenever practical.

Principle 2.0 The development standards must be implementable

2.1 The cost to construct and install the infrastructure contemplated within the Design Standards should not be so great as to discourage or otherwise negatively impact new development.

2.2 The development standards should entail common construction practices and materials.

2.3 There should be a single point person or organization administering the Design Standards.

2.4 There must be a method for holding all parties accountable for the proper installation and ongoing maintenance of the infrastructure.

Principle 3.0 There must be a balance between initial cost and long term maintenance

3.1 The initial installation and eventual replacement costs should not be so great that they discourage development.

3.2 It is important that the materials and products specified have low maintenance costs and reasonable lifespans, as defined jointly by the Downtown stakeholders.

Principle 4.0 Design standards should be unique but also consistent

4.1 The Design Standards should create a streetscape that differentiates the Downtown area from the rest of the City.

4.2 Design standards should be “timeless” and not associated with any fad or particular point in time.

4.3 A best practice for keeping streetscape maintenance costs low is uniformity and simplicity; particularly in materials and products such as lighting fixtures and bulbs, irrigation systems, trash receptacles, street furniture, and paving materials.

Principle 5.0 There must be an equitable sharing of costs

5.1 The infrastructure contemplated within the Design Standards should benefit both the adjacent property owner and the community at large.

5.2 The Design Standards should clearly state which parties are responsible for the initial construction, maintenance, and replacement of the various infrastructure elements.

Principle 6.0 Streetscape infrastructure should benefit as many people as possible

6.1 The Design Standards should contain standards that meet or



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exceed current accessibility requirements, and that embrace the concept of “Universal Design”.

6.2 Design standards should benefit the adjacent property owner while emphasizing the pedestrian experience (walkability).

6.3 Design standards should “enhance public safety and create inviting streets and public places for people” (ReImagine Reno Master Plan GP 3.2).

6.4 The Design Standards should contemplate that the infrastructure may at various times be used for business promotions, festivals, parades, and other activities.

Principle 7.0 Trees and other vegetation are important to the long term success of Downtown

7.1 Trees and other vegetation should be selected and installed in such a way as to enhance the pedestrian experience, improve the ambient environment, promote economic development and tourism, and ensure plant health and longevity.

Principle 8.0 Design standards should support and implement the community vision

8.1 The Design Standards should implement the community vision as set forth in the ReImagine Reno Master Plan, the Downtown Action Plan, and the Urban Land Institute (ULI) Virginia St Corridor Advisory Services Panel Report.

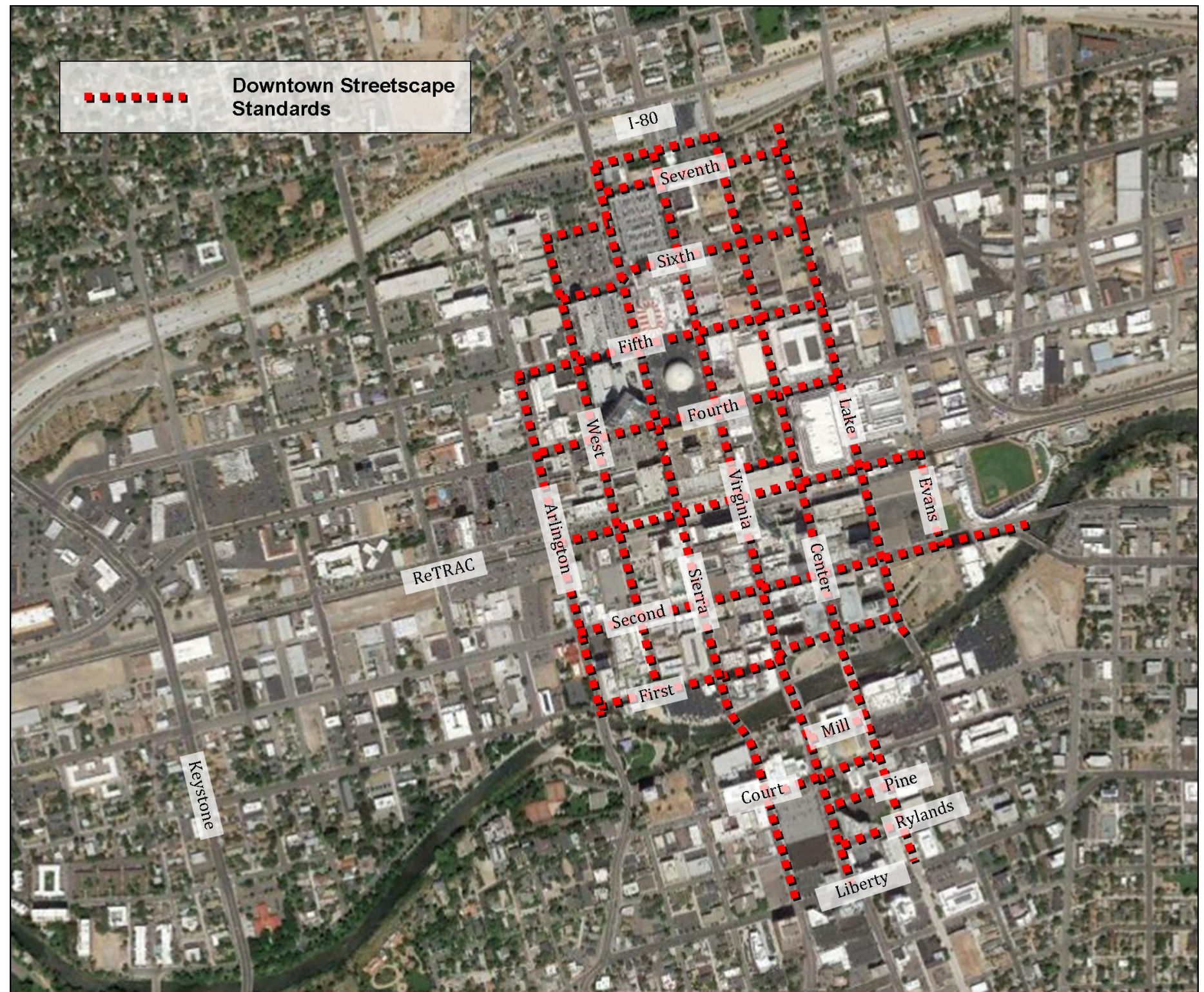
Streetscape Implementation

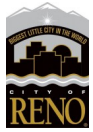
The City of Reno Department of Public Works is responsible for approving all improvements within a public right-of-way, but may at times delegate some or all of its responsibilities to other Departments or entities. Regardless, the City Engineer reserves the right to determine the appropriateness of any proposed improvement to a public right-of-way designated within the Design Standards. A list of installation, maintenance, and replacement obligations is found in the Appendix.

Applicability

The owner or developer of any property adjacent to a section of

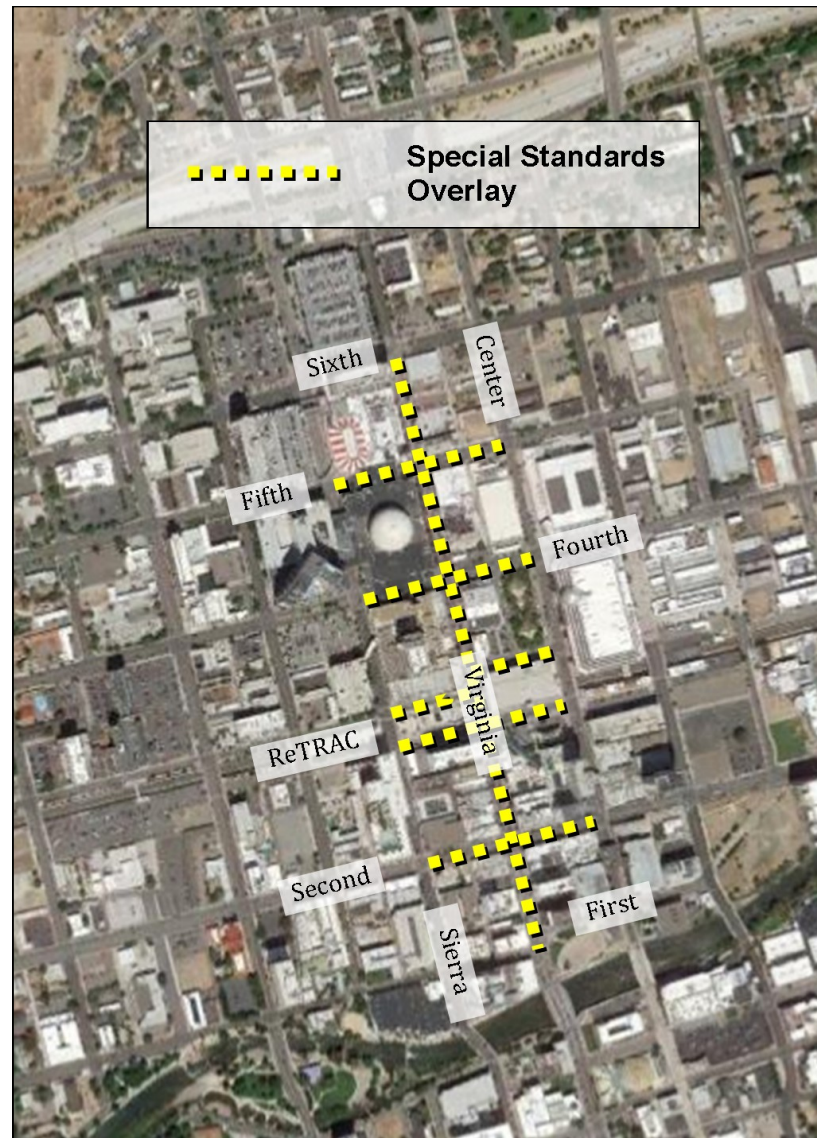
Figure 01 — Downtown Streetscape Design Standards Map





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Figure 02 — Special Standards Corridor Map



public right-of-way designated on the Downtown Streetscape Design Standards Map or Special Standards Overlay Map shall construct all of the applicable improvements as set forth in the Design Standards:

1. Whenever new construction is proposed on a vacant or demolished site.
2. Whenever more than 10% of an existing site or total building floor area is added. However, improvements are not required

for an addition of 500 sq ft or less to an existing structure.

This shall not abrogate or limit the ability of the City, County, or other governmental entity to form a special assessment district and levy property assessments as set forth in law.

Furthermore, the standards set forth herein shall apply to both sides of any section of public street or right-of-way designated on the Downtown Streetscape Design Standards Map except as follows:

1. Bridges shall be designed and improved in a style complimentary to the design standards contained herein and other bridges in the downtown area,
2. Special Standards improvements, if adopted, shall be constructed on both sides of a street or right-of-way designated on the Special Standards Overlay Map.

Any project subject to the standards contained herein shall incorporate a landscaping plan that includes street trees, prepared by a licensed Nevada Landscape Architect who shall certify that the landscaping plan is designed and executed in accordance with the requirements contained herein and other applicable City ordinances.

Alternative Designs

The City Engineer may grant a deviation from any construction standard set forth herein if all of the following conditions are met:

1. The hardship condition must be the result of a physical site constraint or be the result of conflicting regulations; and
2. The alternative design must meet the intent and functionality of the adopted standard; and
3. The alternative design must be of equal- or higher-quality materials than the adopted standard.

Conflicts and Ambiguities

When reviewing and approving any proposed streetscape design, the emphasis shall be on pedestrian safety and access. Any conflicts or ambiguities shall be evaluated based on the eight (8)

guiding principles of the Downtown Streetscape Design Standards.

Streetscape Maintenance

Within the Appendix is a non-exhaustive list of streetscape improvements and the parties responsible for their maintenance. After review and approval by the City Engineer, this list may be updated and any responsibilities herein reassigned or otherwise modified through maintenance contracts, the creation of a special assessment district, and any other lawful manner.

Streetscape Design Details

All improvements shall be constructed in accordance with the applicable design and construction standards set forth in the most current version of Reno Municipal Code and the City of Reno Public Works Design Manual, except as they may be modified herein. A list of the details specific to this Downtown Streetscape Design Standards are found in the Appendix under *Standard Details for Public Works Construction*. Construction and design details may be periodically updated as necessary. All plans shall incorporate the most current version of any applicable design/construction detail, except in limited circumstances where it is determined by the City Engineer that a previous standard is appropriate to repair or complete existing improvements.

Specific Downtown Streetscape Standards

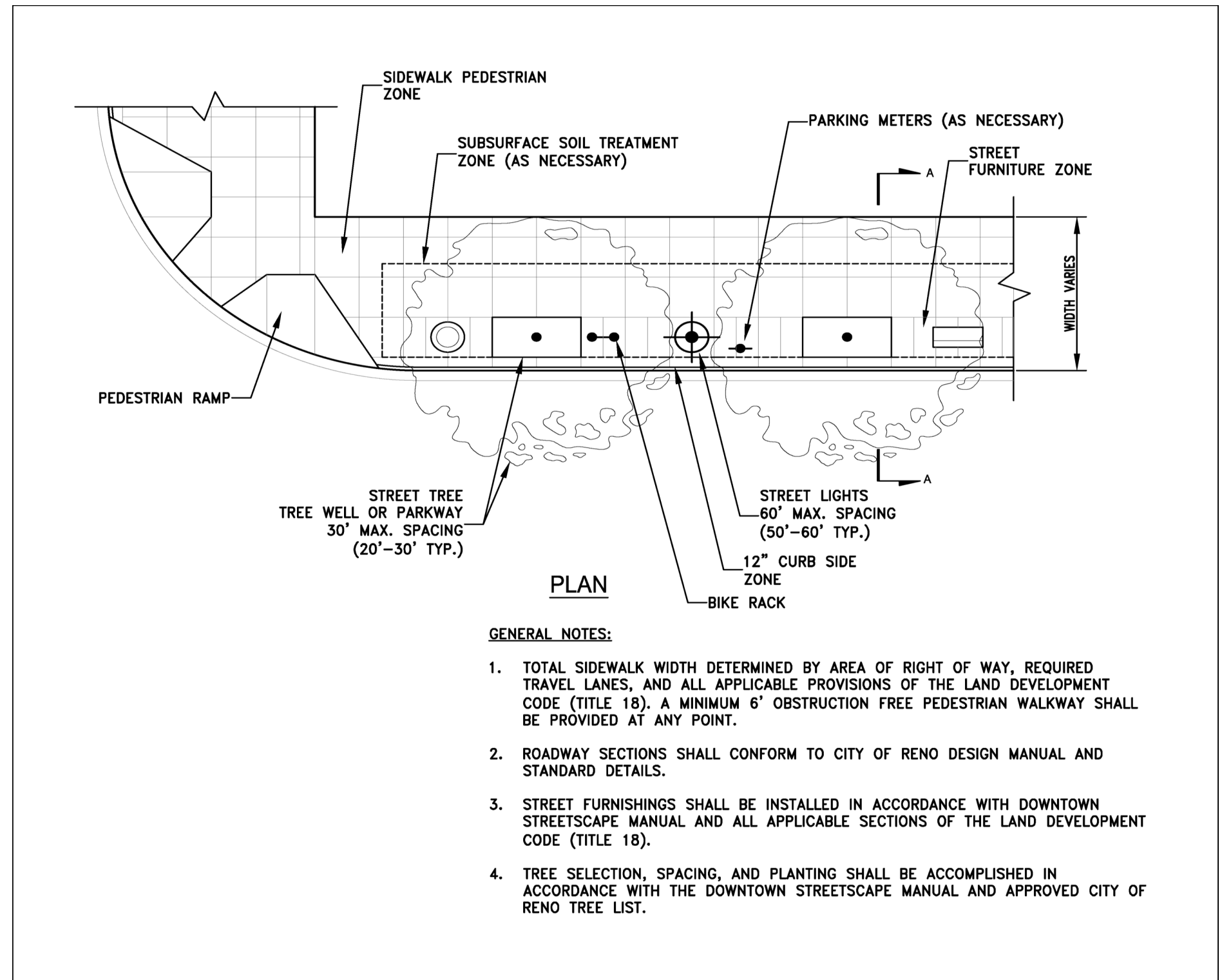
1. All sidewalks shall have a minimum 6' wide unrestricted pedestrian travel way at all points. Street furniture, light poles, hydrants, utility boxes, and any other elements or features shall be designed and installed in such a way as to preserve the minimum unrestricted pedestrian travel way.
2. All sidewalks shall be finished in solid color concrete with a jointing pattern as set forth in Figure 03 and the *Standard Details for Public Works Construction* detail listed in the Appendix. Alleyways shall be constructed of solid color concrete matching the sidewalk, but finished and rated for service vehicles as set forth in *Standard Details for Public Works Construction*.



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3. Landscaping shall be selected and installed in accordance with the appropriate design details contained herein based on location, solar exposure, available planting space, street function, and other site specific factors.
4. Street trees shall be selected from the approved City of Reno street tree list contained in *Standard Details for Public Works Construction* and referenced in the Appendix, based on the location, solar exposure, canopy area, required root volume, and related site specific elements. The landscape architect of record, in consultation with the urban forester, shall determine the appropriate size and species of tree given the site specifics. The landscape architect of record shall select a species from the largest category on the approved street tree list (Canopy class 4 at time of adoption) unless the conditions require a smaller tree species, in which case the landscape architect shall select a tree from the largest category suitable for the location. Criteria for evaluating appropriate tree size include, but are not limited to: right-of-way width, utility conflicts, clear-view requirements, regulatory signal and sign locations, and any adopted public safety standard.
5. Irrigation valves, timers, and other equipment servicing street trees and related landscaping shall be installed in a location readily accessible to City of Reno employees or their agents, and shown on any plan submitted for approval.
6. Trees and tree wells shall be generally centered between, and inline with, street lights to maximize light coverage. Trees shall be generally installed at intervals of 20-30'. Tree wells shall be of a sufficient size to accommodate the proposed species of trees at maturity - see Figure 04. Each tree well along Virginia Street shall include an energized 120 v outlet.
7. Streetlights shall be generally installed at intervals of 50-60' and inline with tree wells.
8. Street furniture shall be installed in accordance with the appropriate design details listed herein based on location, available installation space, street function, and other site specific factors.
9. Significant art features and related decorative elements shall be reviewed and approved based on the location, available

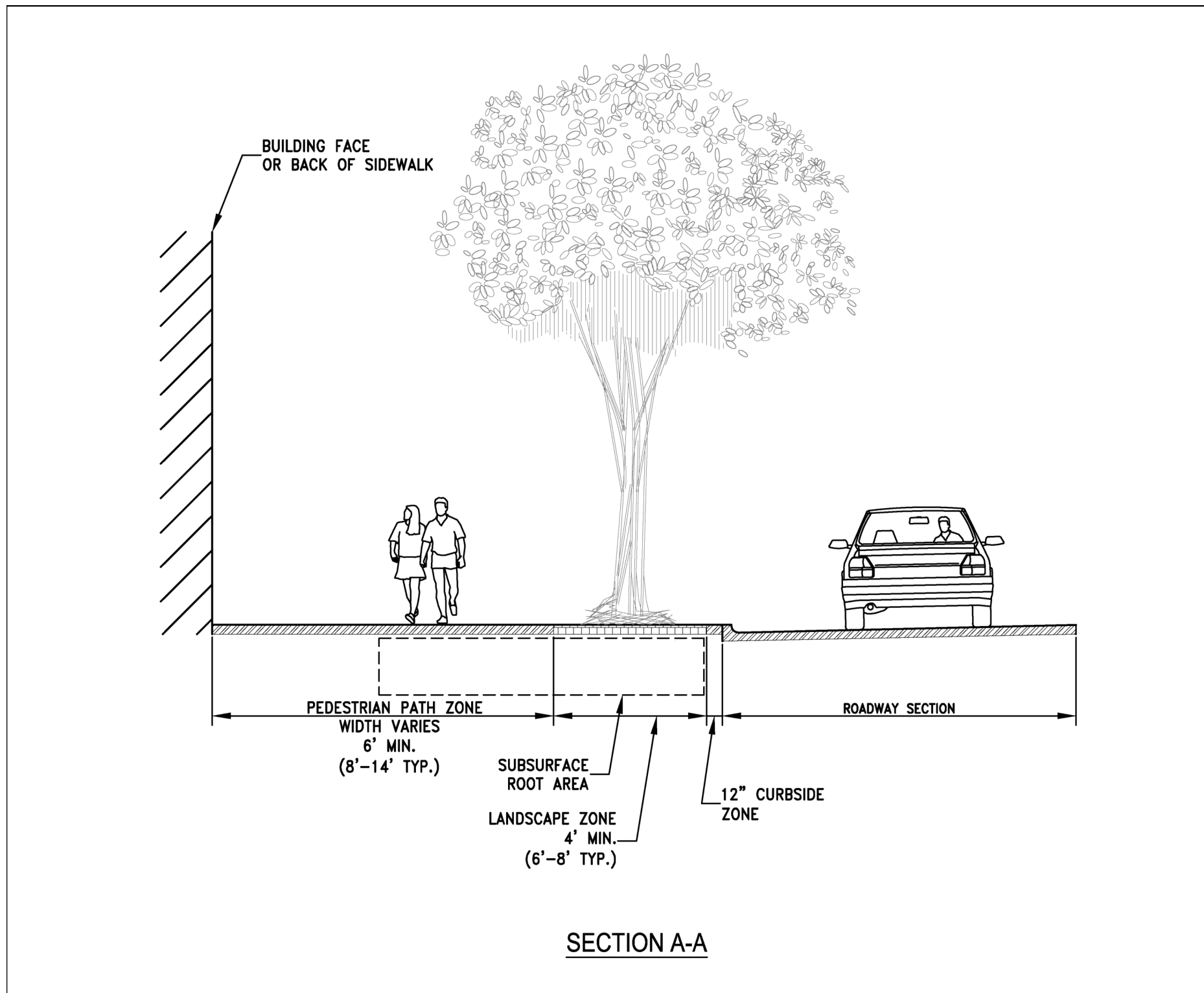
Figure 03a — Typical Urban Street Configuration Aerial View





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Figure 03b — Typical Urban Street Configuration Cross Section



installation space, street function, a review by the Arts and Culture Commission, and other site specific factors.

10. Special Standards Corridors may include improvements unique to those areas. Furthermore, the City or its agent may install optional infrastructure such as fencing, event power pedestals, safety barricades, or lighting in conjunction with the standard infrastructure required of a developer or property owner. A non-exclusive list of infrastructure details is found in the appendix.
11. A streetscape configuration shall be generally consistent for the entire length of the block, except as necessary to address site specific anomalies and to address microclimate issues such as solar exposure and soil conditions.
12. An applicant, at his/her sole discretion, may apply a portion or all of the 1% project cost pedestrian amenity requirement es-

Figure 04 — Tree Well Size

Sidewalk width (TBC to Prop. Line)	Minimum tree well size
< 15'	4' x 8' Tree well with grate
15' - 20'	5' x 10' Tree well with grate or curb
> 20'	6' x 6' Tree well with grate or 6' x 10' tree well with curb

tablished in RMC Title 18 toward street tree root volume capacity and related improvements that facilitate and promote the installation, health, growth, and longevity of larger tree species, and/or toward the expense of installing electrical service for tree lighting.

[End of Section]



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Appendix

Construction Standards and Details

Street Furniture

- Bench: Site specific review and approval by City Engineer and Redevelopment Agency Manager
- Bike Rack: COR R-605 or alternate approved by City Engineer and Arts and Culture Committee
- Trash Receptacle: “Bigbelly” model HC5 Single Station or alternate approved by City Engineer

Infrastructure

- Decorative Colored PCC Sidewalk: COR R-104B, R-104C, R-104D, R-104E, R-104F, R-104G, as applicable
- Tree Wells: COR R-501A, R-501B, R-501C, 501D, as applicable
- Tree Drip Irrigation System: COR R-520A, R-520B, as applicable
- Decorative Street Light with or without Traffic Signal: COR R-413B, R413C, R-413D, as applicable
- Decorative Bollard: R-603B
- Tree Species: Urban Tree Details and Tree List, COR R-500A through R-500J
- Planting Soil Detail: Planting Soil Details COR R-500K, R-500L

Streetscape Maintenance Responsibilities

- Art Feature: Owner/Applicant
- Street Furniture (benches, bike racks, etc): Owner/Applicant
- Street Light Installation (new): Adjoining property owner

- Street Light Maintenance/Power: Reno Dept of Public Works
- Landscaping, Irrigation, and Street Tree Installation (new): Adjoining property owner
- Landscaping, Irrigation, and Street Tree Maintenance and Replacement: Reno Dept of Parks and Recreation
- Sidewalk Installation: Adjoining property owner
- Sidewalk Replacement: Adjoining property owner
- Sidewalk Sweeping/Cleaning/Snow Removal: Adjoining property owner
- Trash Cans: Adjoining property owner/Receptacle owner